

FLIMZIE

*The Newsletter of the Rock River Valley Division
Midwest Region, National Model Railroad Association*



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The Rock River Valley Division, RRVD, is a local division of the Midwest Region of the National Model Railroad Association, NMRA. The RRVD serves NMRA members in areas of Green and Rock Counties of Wisconsin, and Boone, Jo Davies, Lee, Ogle, Stephenson, Whiteside, Carol, DeKalb and Winnebago counties in Illinois. The RRVD holds monthly meetings typically the first Sunday afternoon of each month, September through May. Starting in September, we will be meeting at the **Paulson's Agriculture Museum, 6950 Belvidere Rd, Caledonia, IL 61011**. The meetings consist of various clinics on model railroading, model contests, and drawings for door prizes for NMRA members. All are welcome. The meeting starts at 1:00 PM, and the doors open at 12:30 PM.

2024 BoD & Chairman Directory

Please use the following address to contact the RRVD organization or any of the following officers:

Rock River Valley Division, NMRA
PO Box 2213
Loves Park, IL 61131-2213

<p>Superintendent: Marty Hendrickx 815-978-7326 superintendent@rrvd-nmra.org</p> <p>Paymaster: Myron Peterson ructic@gmail.com</p>	<p>Assistant Superintendent: John Mann asstsuperintendent@rrvd-nmra.com</p> <p>Chief Clerk: Steve Faivre 815-761-5070 smfaivre@gmail.com</p>
<p>Director: Gary Loiselle, MMR 815 / 963-8326 hofreight@gmail.com</p>	<p>Director: Doug Loy 815 / 765-2046 delsclho@yahoo.com</p>
<p>Director: Ken Peterson 779 / 348-2961 poplarken53@gmail.com</p>	<p>Director (Past Superintendent): Ray Dyreson 815 / 335-7046 815 / 494-0205 raydyr@aol.com</p>
<p>Show and Sale Chairman: Doug Loy 815 / 765-2046 delsclho@yahoo.com</p>	<p>Flimzie Editor: Ken Peterson 779 / 348-2961 poplarken53@gmail.com</p>

<p>Membership Chairman:</p> <p>Ken Mosny 815 / 965-4871 kenmosny@gmail.com</p>	<p>Clinics Chairman:</p> <p>Ken Mosny 815 / 965-4871 kenmosny@gmail.com</p> <p>John Mann asstsuperintendent@rrvd-nmra.com</p>
<p>Contest Chairman:</p> <p>Ray Dyreson 815 / 494-0205 raydyr@aol.com</p>	<p>Layout Tours Chairman:</p> <p>Bert Morris 815 / 520-1307 winxxx1115@gmail.com</p>
<p>Division Publicity Chairman:</p> <p>Joe Whinnery 815 / 398-8973 phowhin@yahoo.com</p>	<p>Nomination and Election Chairman: Advancement Chairman:</p> <p>Gary Loiselle, MMR 815 / 963-8326 hofreight@gmail.com</p>
<p>Show and Sale Registrar:</p> <p>Ken Mosny 815 / 965-4871 kenmosny@gmail.com</p>	<p>Chief Dispatcher/Facebook:</p> <p>George Stark george-nmra@bombadil.org</p>

Flimzie Deadlines

The Flimzie is published once per month on the first of the month. It will be placed on the RRVD website for anyone that wants to read it.

The content for the Flimzie comes from you, our readers. Please submit your articles, pictures and editorial comments to the Editor, Ken Peterson, poplarken53@gmail.com, no later than 10 days before the 1st of the month, i.e., Jan 21, 2024, for a Feb 1st, 2024, publication.

From the Editor
By Ken Peterson

Friends of the Flimzie, I need to point out that this publication is for you and by you. All the articles that appear here are written by members of the RRVD. If you don't submit articles for publishing, there is no content. When I took over as the editor, I wanted to change the Flimzie from a quarterly publication to a monthly one. Without articles from you, I may have to switch the Flimzie back to quarterly, or even just occasionally, based on when I get content. This will be completely up to you.

What Are Friends For? Chapter 36
Gary D. Loiselle

Let's start this chapter by back-tracking a bit. In chapter 29, I mentioned several RRVD members who have been 'published' in some way/shape/or form in MR. They included Jim McQueeny, Dick Caudle, and Joe Whinnery. I didn't mention at the time that Joe had been the RRVD photographer-of-record for as long as I can remember. What some of you may not be aware of is the fact that Joe is also one of the Rockford Park District's trolley operators. **(Photo 1) WAFF**

Back on track (pun intended), there are two more articles from Model Railroader Magazine that I would like to draw your attention to with regard to 'structures' stuff. The first, IT'S THE LITTLE THINGS THAT COUNT, by Michael Tylick, Dec. 1991, page 100 **(Photos 2A, 2B)**. The second, DETAILS DO MAKE THE DIFFERENCE, by Art Curren, Dec. 1992, page 138 **(Photo 3)**. I don't know about you, but I would whole-heartedly agree with both authors. I cannot say whether my appreciation of details is because of my interest in model railroading or because I have become more detailed orientated due to my association and long-time friendship with RRVD member Tom Maladecki. For those of you that know Tom, you must agree that his eye for detail is unparalleled. (LOL, WAFF).

The search for 'details' has also provided me the opportunity to learn some things about Rockford history that as a non-life-long resident I might not have any idea about. For example, there is a building on Charles St., just west of 20th St., that is now an Army/Navy surplus store **(Photo 4)** (2024 Photo). There is an interesting detail on the east side of the building (my opinion, anyway). There are two places where it looks as though the brick has been repaired or replaced **(Photo 5)**. As luck would have it, in my collection of structures from around town, I found an undated slide/photo of that exact building **(Photo 6)**. My curiosity led me to stop in and talk to the owner. Interesting. He told me that this was the original place where Testors manufactured glue. How many of you model builders have not used Testors? The large tube down the side of the building was a fire escape. Interesting detail. WAFF! The owner explained that kids in the neighborhood would climb up the tube and workers inside would run water down to make things 'slide' better. (Talk about "*What Are Friends For*")

I told the owner about my undated photo of the building, and he asked me to e-mail a copy of it so he could print it out and display it. I have not stopped back in to see if it has been done yet.

I also went down to Buckbee St. and got some photos of what I knew as "Testor's" **(Photo 7)**. I included a copy of my 2024 photo in my e-mail.

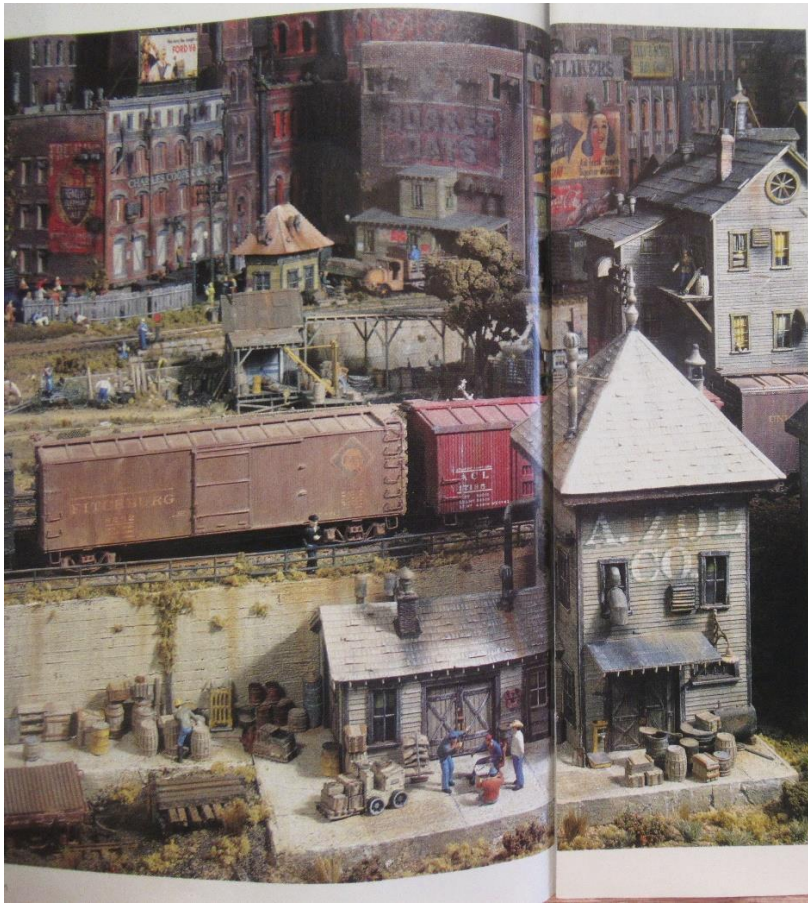
I am probably dating myself when I tell you that I still subscribe to the Rockford Register Star newspaper. We also still have a land-line phone. For you non-subscribers, in the Tuesday, June 4, 2024 edition of the paper the front page story featured a photo of the Buckbee St. Testor's building. **(Photo 8)**

Information gleaned from the article: The building was originally constructed between 1897 and 1905. It was originally three stories. The fourth and fifth stories were added between 1913 and 1928. Of most interest to me, the building was originally part of the H.W. Buckbee Company, a mail-order wholesale seed and plant distributor. Testors moved into the building in 1944. According to the article, it was occupied by their Woodworking Division known for scale pine-wood model airplanes. Testor's left in 2005.

The building was acquired by a developer with thoughts of creating low-income apartments. The article states that one reason the project didn't move forward was due to a working railroad track behind the building. I really can't see that as a problem. How about you? WAFF. **(Photo 9, 2012 photo from CN side of building during track work to bring CP Rail to its track to Davis Jct. after the Milwaukee/SOO bridge across the Rock River was converted to a pedestrian walkway) (Photo 10, 2024 photo shot from Seminary St. bridge by Joe Behr).**



Photo 1



It's the little things that count

Careful attention to detail makes scenes come alive

BY MICHAEL TYLICK
PHOTOS BY THE AUTHOR

GEORGE SELLIOS has built one of the most delightful model railroad scenes of recent times. Aside from the vastness and artistry of the completed city portion of the HO scale Franklin & South Manchester (featured in the March 1990 MR), one of the things I enjoy most is George's careful attention to the smallest details.

His city of Manchester abounds with hundreds of small, every-day events and stories — so many that the eye flits from one to the next. The effect is much like looking out the window of a train: tiny slices of life flash by to be quickly digested and replaced by the next.

Photos isolating very small areas of the F&SM are truly the best way to appreciate the attention lavished on every square inch of an enormous layout. Although the F&SM is a very large private layout, all the scenes here could fit on even the smallest layouts.

THE A. ZOLCO is an interesting structure. The tall square tower with its hip roof finally looks like it originally had a different use. We can only surmise what it George has left to our imagination what A. Zol actually makes, but judging by many wood barrels and crates, their product looks small but heavy. The forklifts, trucks, and wagons are needed to move them. Judging by the number of roof vent ductwork, it also appears they deal with many toxic or dusty materials. Instead of "one-of-each" types of platform clutter, George has wisely made most of the packing similar: after all, a small industry has a limited product line. But a scale, a few drums, and some trash barrels give some variety to the scene.

The placement of the figures is also nicely done. A number of people are clustered on the main platform. The man with the cowboy hat, perhaps the foreman, appears to be discussing a problem with the workers, who are obviously on a break. The break problem, and the man just above the rail bumper has decided he'd better look while the worker at the far left either didn't get the message yet or else figures he's out of view. Across the tracks, there are many more figures and vehicles telling the stories. One of the greatest joys in visiting the F&SM is that the areas three feet from the aisle are just as intensely detailed as the foreground.

George has been wise to treat different parts of the scene differently. Various coloring, scale (size of structures), signs, and rooflines make this a photo that studied for hours, with new lessons to be learned with every look.

MODEL RAILROAD

Photo 2A

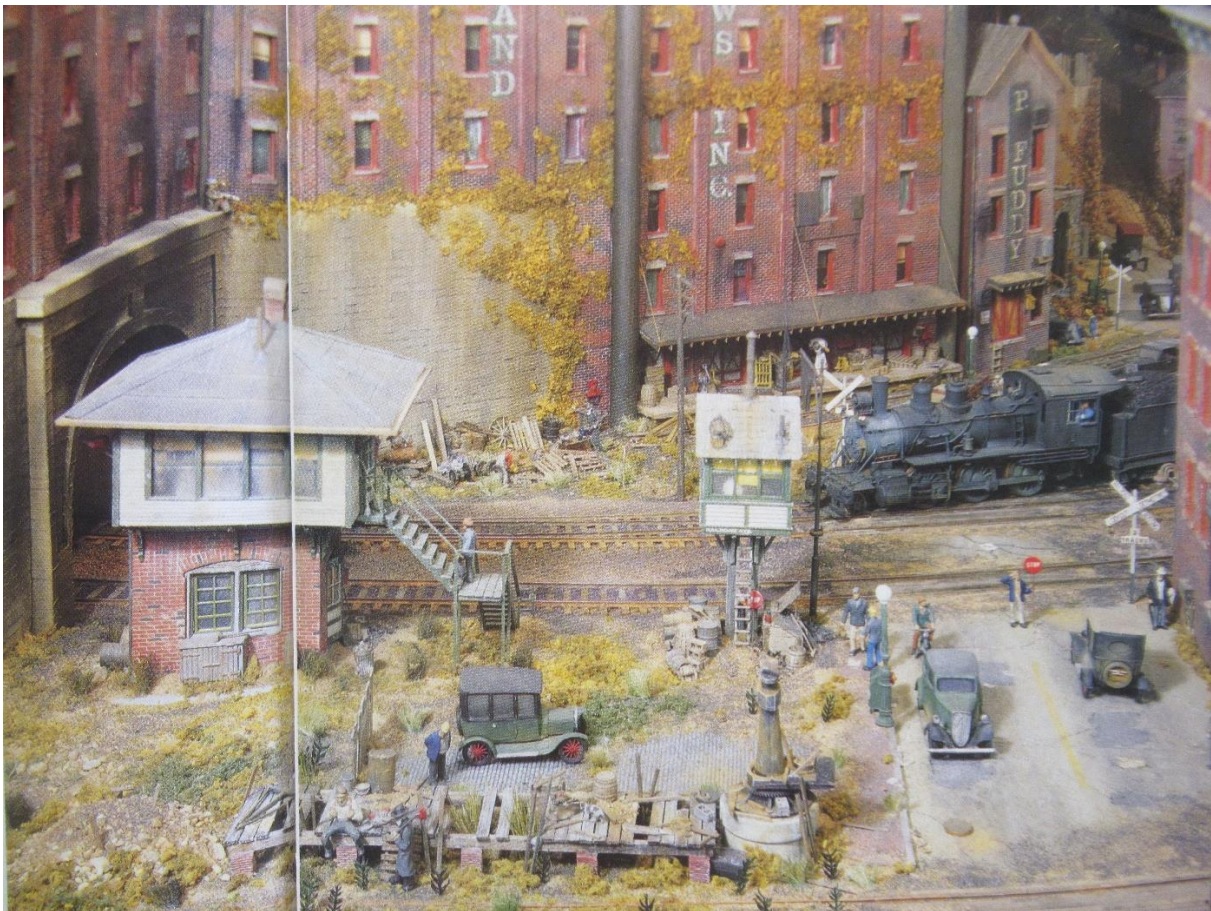
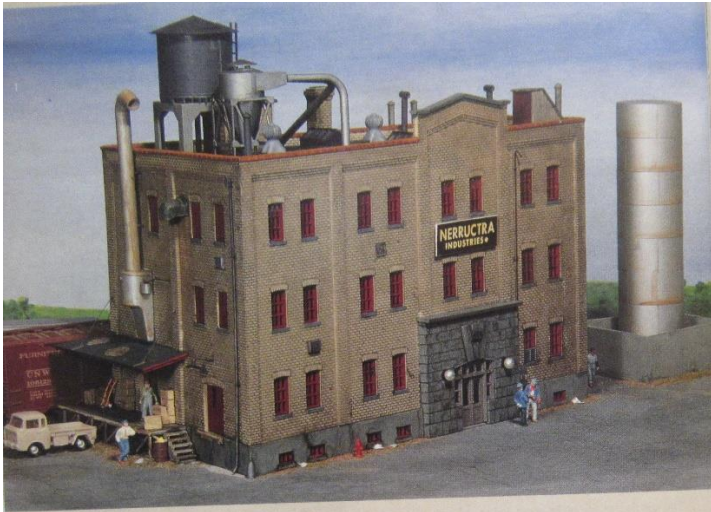


Photo 2B



With the addition of a few details, this Walther's HO scale Reliable Warehouse & Storage kit takes on a new personality.

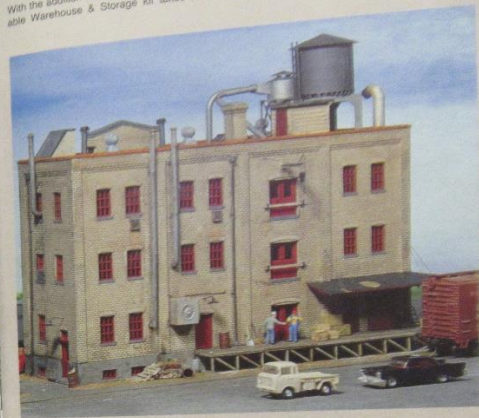


Fig. 2 REAR-WALL DETAILS. The platform roof, vents, piping, and lamps add life to the walls.

Details *do* make the difference

Using sprues, wire, plastic, and commercial parts to dress up a common kit

BY ART CURREN
PHOTOS BY THE AUTHOR

THE DIFFERENCE between an average model and one that's striking is often the added detail. It's not necessarily the amount of detail, but the quality and choice. In the case of a structure, it's often the small things that make it more realistic.

Recent kit releases have featured a wide variety of structure styles and sizes. I chose the new HO scale Reliable Warehouse & Storage kit from Walther's to illustrate how details can make a

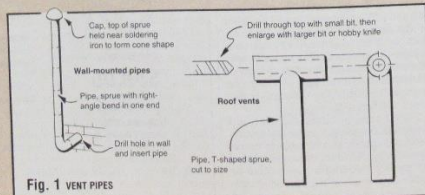


Fig. 1 VENT PIPES

typical building look better. I like this kit because it's large enough to be a bona fide industry, unlike some structures that are too small to generate any real freight traffic on a model railroad. The structure's shape is unique, giving it a special appeal. It also has a few nice details of its own, such as the water tank, rooftop entrance, and loading dock.

ASSEMBLY

Before we can add any details we have to assemble the kit. It goes together easily with one exception. The roof is designed to fit into a groove on the inside of the walls below the line of molded-on bricks. However, the nicely molded tile caps atop the walls are wider than the walls, making it difficult to insert the roof over the tiles.

I prefer removable roofs since I weather a building before adding window material and a removable roof makes this easier. My remedy was to slice a scale 15-wide piece off the wide end of the roof and bond it directly to the wide side wall.

This allowed enough room to slide the rest of the roof in place easily. The old alternative would be to glue the entire roof in place, but the choice is yours.

FREE DETAILS

Details for free? Wow! I'll go for them. One freebie is cracks added to the foundation. Just scribe them in with a knife blade. Most cracks occur below the vertical pilasters or at weak points, where the basement windows break the solid wall.

Other free details come disguised as the sprues the kit parts are attached. *Don't throw them away!* Some kits have better sprues than others, and this is blessed with good ones. I used longer ones as vent pipes on the end and short side walls, as shown in Fig. 1. Some sprues had 90-degree elbows one end, and I inserted these in the drilled in the walls. If the sprue does have a 90-degree bend, you can add a piece off the wide end of the roof and bending it while it's soft.

Photo 3



Photo 4



Photo 5



Photo 6



Photo 7



Photo 8



Photo 9



Photo 10
WAFF, Gary

The Layout Design Column #14

By Ken Peterson

Construction

The last three columns I told you what steps were used to complete the design of the **Minneapolis Mill and Elevator Corridor Railroad (MMEC RR)**. This month I will describe the construction of the foamcore base the layout will be assembled on. To start, here is a list of materials and tools (**See Photo 1**):

- 30" x 40" x 3/16" thick foamcore panels I ordered from Amazon, it came 10 per box
- Wood glue (waterproof)
- Hot glue, hot glue gun
- Right angle squares of different types and sizes
- 4ft metal straight edge (I used my level)
- Tape measure
- Pencil
- Mat board cutter
- Exacto knife, new #11 blade
- Dimensioned drawings
- Garage door handles
- Scraps of Masonite for reinforcement
- (4) 1 x 2 x 80" pine lumber
- (12) 2 x 2 3-5/8" pine lumber

First, I laid out and cut all the required pieces from five foamcore panels. All scraps were saved to be used as reinforcement, filler pieces, and mock structures. I used two methods to glue the pieces together. I formed four basic rectangular bases 14" x 40" with fillets of hot glue in the corners. Two joists were evenly spaced to stiffen the long sides. The long sides and the ends were laminated two layers thick. All the laminations were done with wood glue. The 2 x 2 pine corner reinforcements came with sanded round corners. The rounded corners provided clearance for the hot glue fillets in the corners. The 2 x 2's were wood glued and fit into the corners tightly. Two 40" modules were glued together to make an 80" module. The two 80" modules are bolted together to make the final 160" layout. There are two 40" long one-track fiddle tracks clamped on each end to act as staging during operations.

As I cut out the foamcore pieces required, I found I made mistakes. The tolerances I wanted to build to were just too tight for me to hand cut with a straight edge and Exacto knife. I had to go to Hobby Lobby and pick up more foamcore. During construction, I found that the bottom edges were not robust enough to slide around when moving the entire assembly. So, I bought and attached the 1 x 2 x 80" pine boards to the bottom outside edges. This will make it much easier to slide the modules into and out of my van without damage. I mounted the garage door handles to make it easy to move the modules.

When I finished the first 80" module, I took it outside to spray paint. The backdrop was painted sky blue on the front and black on the back. The base was painted all black except for the top. The top I wanted to paint with a rock/pebble textured spray paint. I had seen this done on a layout in a video. It was used as foundation to ballast and dirt scenery. It looked great in the video. I sprayed it on and left it to dry on one of those 90deg days in August. That was a big mistake. The paint delaminated the top layer of paper/cardboard on the foamcore. I ended up peeling off the whole top layer. I repainted it with my basic brown latex scenery paint. One of the backdrops didn't like the sun either.

When I painted it, the heat warped it slightly. Rather than rebuilding it, I tried to straighten it by bending it back. That caused a crease. Then I tried laying it on the work bench with heavy weights. It straightened out somewhat. I have until next March to decide if I want to rebuild it before the Show.

Photos 2 and 3 show the two modules bolted together, warts and all. The brown paint is still wet in these pictures.

Next, the track plan will be drawn on the modules. I plotted a dimensioned drawing of each module showing the location of all the track. See **Photo 4**.

I believe track is scenery and should be modelled as such. It occupies most of the layout. As scenery it deserves a great deal of time and effort to make it look realistic. The track will be prepared for installation by modifying it, painting it, and applying preliminary weathering. Pictures of the prototype track show various levels of aging that I will attempt to model. See **Photos 5, 6, 7 and 8**. These older photos show the track was better maintained in the 50's and 60's. You can see all the rails and ties. The more current picture, **Photo 9**, shows badly neglected track. The sidings serving an elevator are buried, while the mainline in the background still has visible rails and ties.

I will take each section of flex track and randomly cut some of the ends of the ties short, and trim other ends off at an angle. I will remove at least one tie every fifth tie and then space the remaining ties out, and force some to be at an angle. My goal is to model this derelict track. I will paint each stick of flex track and the turnouts a basic railroad tie brown. Then the ties will be individually painted as randomly as possible different colors of grays, browns and black. That will be followed by patches of rust on the spikes and tie plates. After the track is ballasted, pastels will be used for the final weathering.

Next month I will show pictures of the track preparation and painting steps, and the track installation.

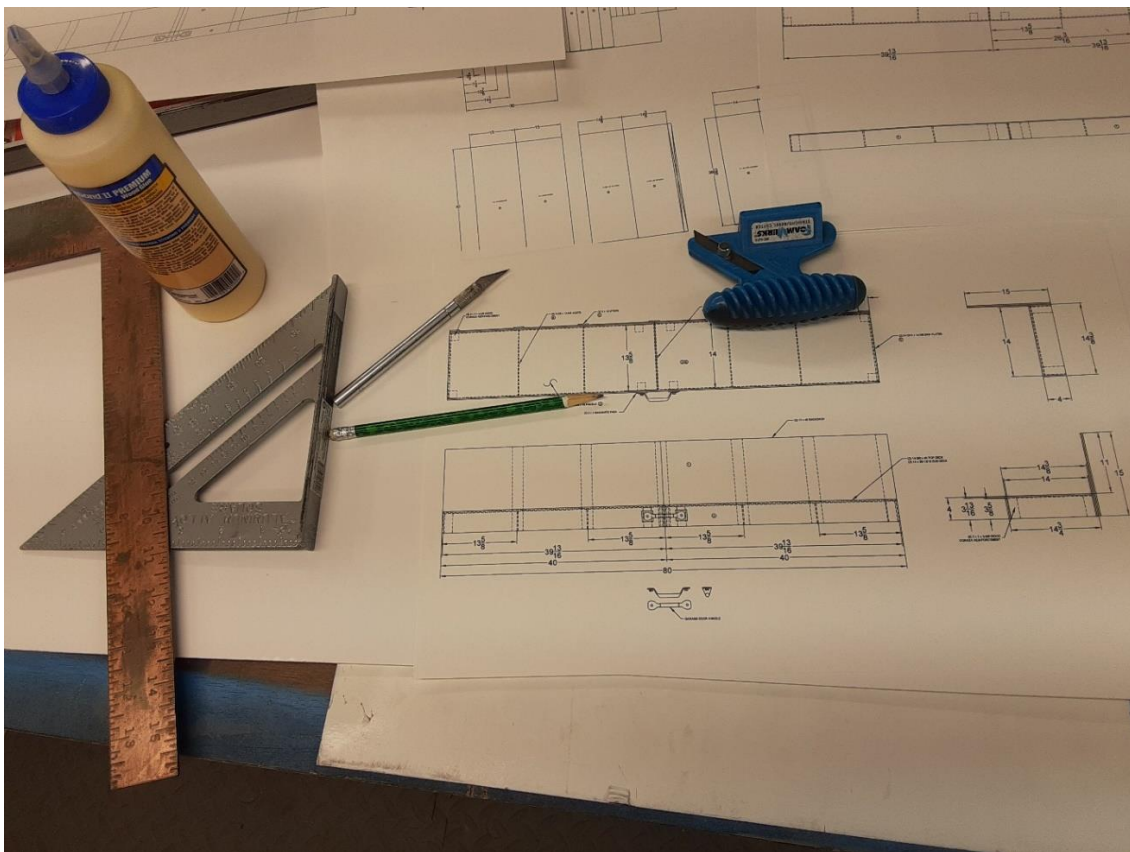


Photo 1



Photo 2



Photo 3

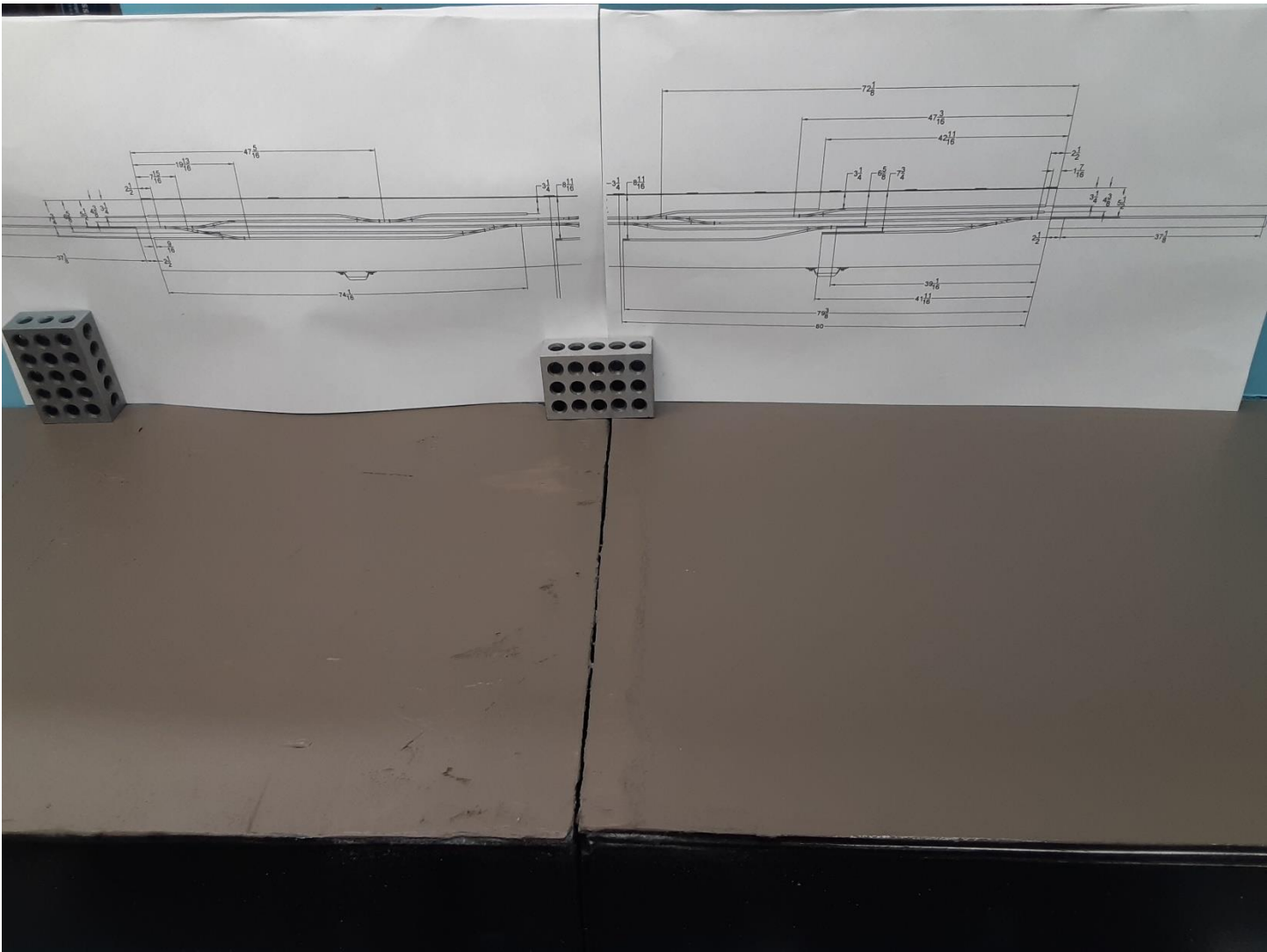


Photo 4



RRPictureArchives.NET Image Contributed by S Kotnour

Photo 5 Hiawatha Corridor 1952



Photo 6



Photo 7



Photo 8



Photo 9
