

# FLIMZIE

*The Newsletter of the Rock River Valley Division  
Midwest Region, National Model Railroad Association*



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The Rock River Valley Division, RRVD, is a local division of the Midwest Region of the National Model Railroad Association, NMRA. The RRVD serves NMRA members in areas of Green and Rock Counties of Wisconsin, and Boone, Jo Davies, Lee, Ogle, Stephenson, Whiteside, Carol, DeKalb and Winnebago counties in Illinois. The RRVD holds monthly meetings typically the first Sunday afternoon of each month, September through May. Starting in September, we will be meeting at the **Paulson's Agriculture Museum, 6950 Belvidere Rd, Caledonia, IL 61011**. The meetings consist of various clinics on model railroading, model contests, and drawings for door prizes for NMRA members. All are welcome. The meeting starts at 1:00 PM, and the doors open at 12:30 PM.

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## **2024 BoD & Chairman Directory**

Please use the following address to contact the RRVD organization or any of the following officers:

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## Flimzie Deadlines

The Flimzie is published once per month on the first of the month. It will be placed on the RRVD website for anyone that wants to read it.

The content for the Flimzie comes from you, our readers. Please submit your articles, pictures and editorial comments to the Editor, Ken Peterson, [poplarken53@gmail.com](mailto:poplarken53@gmail.com), no later than 10 days before the 1<sup>st</sup> of the month, i.e., Jan 21, 2024, for a Feb 1<sup>st</sup>, 2024, publication.

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## **From the Superintendent**

**By Marty Hendrickx**

Hello all! I hope everyone is enjoying their summer. It seems like it is over too soon before we are into fall. RRVD does have one summer event still on the calendar. Wednesday, August 21st RRVD will be having its annual potluck picnic. Ken Mosny sent out the notification the other day as we do ask you to RSVP the event, so we have enough grill meat for everyone. This is a great event for everyone to get together at a social event to mingle with your fellow model railroaders and their significant others. There is no cost to attend other than bringing a dish and a dessert to pass around. I am hopeful we have a great turnout and to see as many of you there as possible.

We do have a change in our monthly meet schedule in September. As most of you have heard the Union Pacific "Big Boy" will be in Rochelle on September 7th and 8th. The 7th is only for UP family, but the 8th is for the general public. As of this writing the details have not been announced, but in anticipation of this event, RRVD has rented a room at the Rochelle Park District to hold our September meet so membership can view the Big Boy either before or after the meet. As information becomes available, we will be sending out information to the membership.

On a very sad note, I just received word that Harold Heeren passed away on July 31st after a long illness. His Toledo, Pacific and Western – 1970 was a joy to operate on and was a very popular railroad with the Rock Rail group. Here is a link to Harold's obituary <https://www.fitzgeraldfh.com/obituaries/harold-heeren>. Please extend your condolences to his wife Judith and the rest of Heeren family.

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## **From the Editor**

**By Ken Peterson**

Friends of the Flimzie, I need to point out that this publication is for you and by you. All the articles that appear here are written by members of the RRVD. If you don't submit articles for publishing, there is no content. When I took over as the editor, I wanted to change the Flimzie from a quarterly publication to a monthly one. Without articles from you, I may have to switch the Flimzie back to quarterly, or even just occasionally, based on when I get content. This will be completely up to you.

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## **What Are Friends For? Chapter 34**

**Gary D. Loiselle**

I ended chapter 33 with a picture of a Gambino Realty sign. **(Photo 1)** That sign is on a vacant lot on N. Main St. in Rockford. It is on the west side of the street between the railroad tracks and the Olympic Tavern. **(Photo2)** And, it is the former site of Essex Wire Co. I only bring this up because

in collection of 'structure' stuff, I have a picture of the unique roof detail that made Essex stick out. **(Photo 3)**

On a similar note, as an Auburn High School teacher, I can't tell you how many times I went by the Amerock plant on Auburn St. **(Photo**

**4 A, 4B)** If you are into roof detail, it couldn't get much better. The plant didn't meet the exact same fate as Essex Wire, but its close.

**(Photo 5)** (December 2023 photo, similar angle)

I would like to interject a couple of ideas about the roof itself. If your kit roof is corrugated steel, a simple paint job of flat silver or light gray might do the job. You could easily add some streaks of rust or soot. If your kit roof (or scratch-built roof) is just a flat piece of styrene, texture can be as simple as a piece of self-adhesive, fine grit sandpaper. **(Photo 6A, B)** Or a piece of tissue adhered with a coat of flat dark gray or flat black paint. **(Photo 7)**

Now, no matter the type or amount of detail on the roof, every roof needs some maintenance at some time. There may be a brief time before repairs can be done that the interior still needs to be protected. **(Photo 8 A)** Ahead of any major repairs, materials need to be assembled to avoid delays once the job is started. **(Photo 8 B)**

On larger flat roof, it may require a whole crew to get the job done. **(Photo 9)** My scene in Photo 9 is a combination of two sets of

Woodland Scenics figures. #785-1828 Roofers, and #785-1822 Policemen. Why both you ask? Because in the movie *Shawshank Redemption*, when the inmates were up on the roof, the guards needed to be there too. LOL Just a reminder "Model Railroading IS fun"! WAFF

The roof of a structure on Dennis Blunt's layout caught my eye and is worth sharing. **(Photo 10)** Looks like the rolled roofing needs some attention. A simple repair or temporary patch can be handled by one man.

One last roof "scene". If you don't have room for an 'adults only' beach, here's an option. **(Photo 11)** (Location to remain anonymous.) I need to confess that this photo 11 is not the original one that I had here. I had a closer shot of the building with the ROOMS sign on the roof. On the roof of THE BELVIDERE there is a swimming pool. There are also several young ladies taking advantage of privacy and the sunshine to work on their all-over tans. I will let your imagination take over so I can keep my photos on a "G" rating. WAFF, Gary



Photo 1



Photo 2



**Photo 3**



**Photo 4A**



**Photo 4B**



**Photo 5**



**Photo 6A**



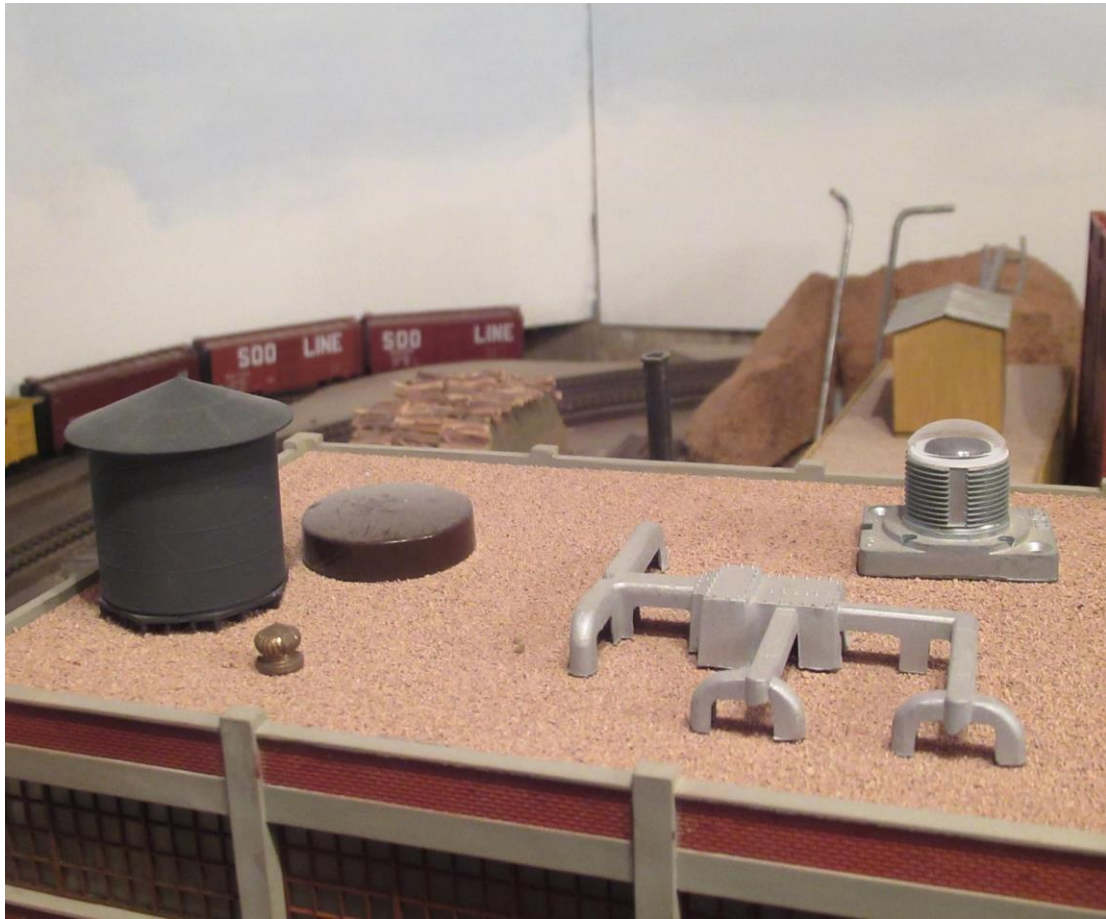


Photo 6B

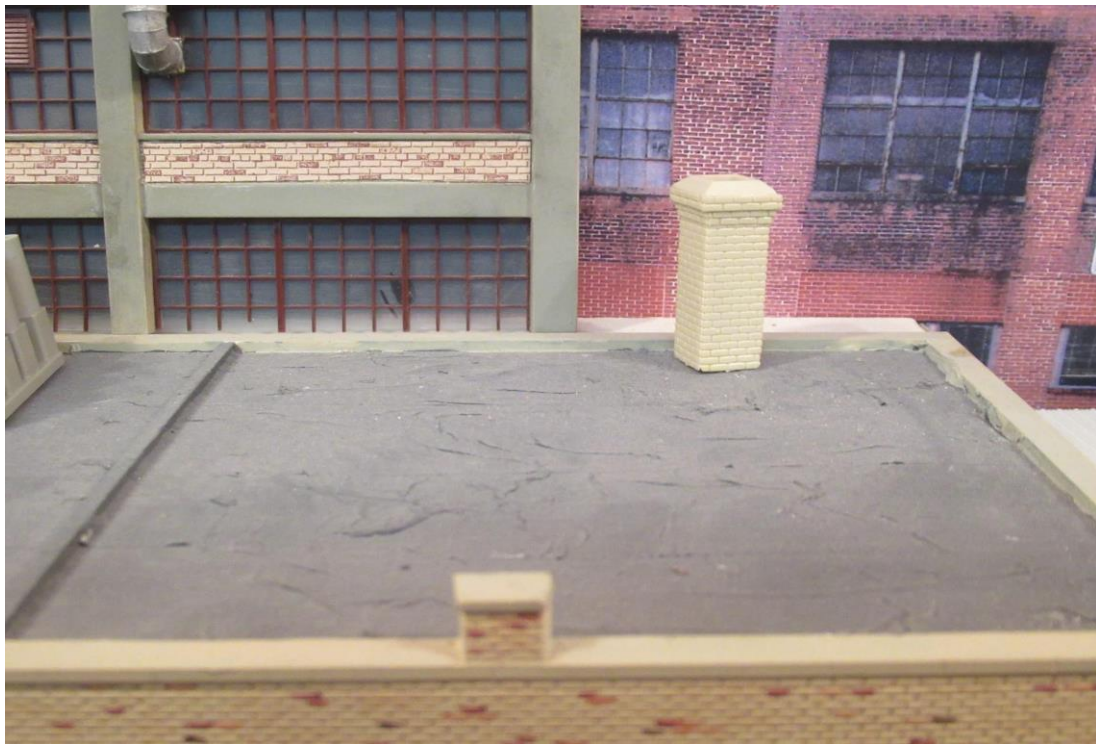


Photo 7



**Photo 8A**



**Photo 8A**



Photo 9

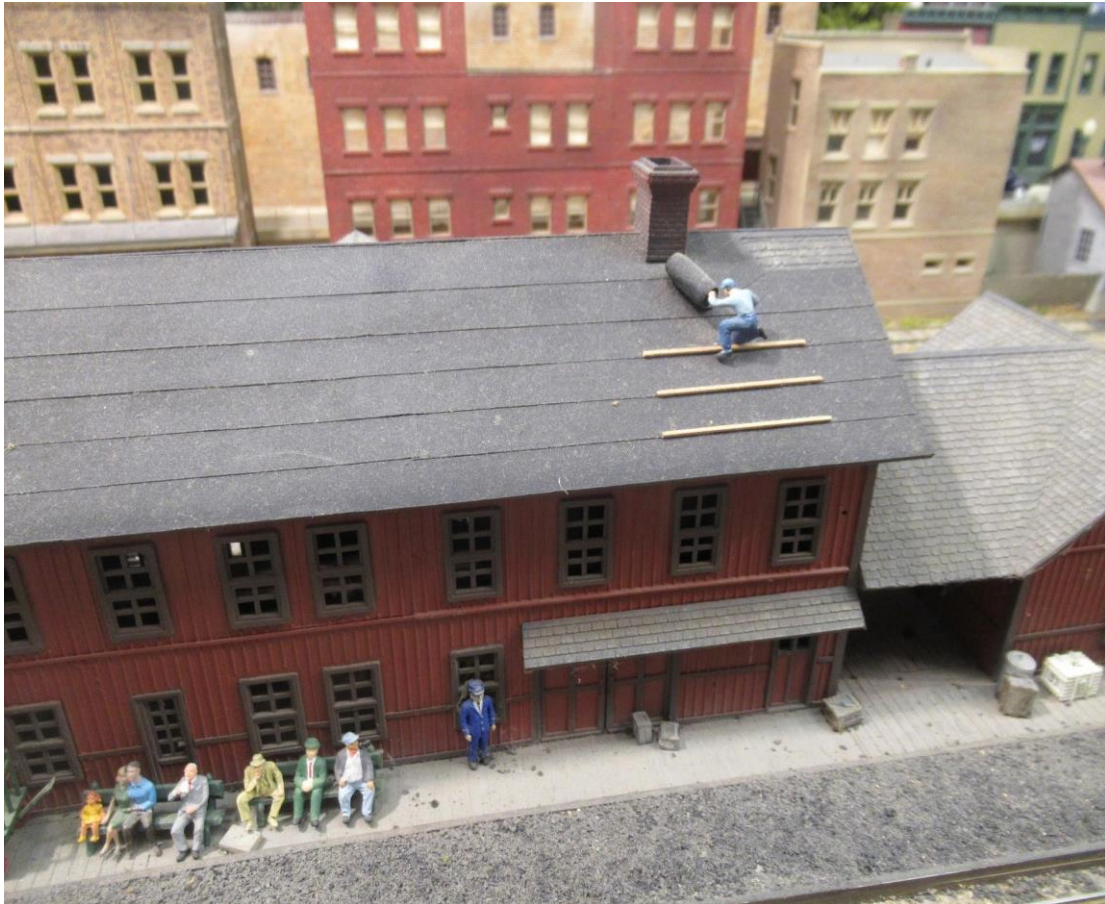


Photo 10



Photo 11  
WAFF, Gary

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## The Layout Design Column #13

By Ken Peterson

### 3. SKETCHES

Last month I talked about the structural approach to the design of **The Hiawatha Mill and Elevator Corridor Railroad**. I discussed how the layout will be constructed, the materials of construction, track and turnouts, the type of rolling stock and the main goal of the layout. For those who did not read last month's column, the main goal of this layout is to demonstrate model railroad operations to those at our 2025 Train Show that are not familiar with operations. A workable footprint was developed, and I sketched ten possible layouts that met that goal. A harsh reality smacked me in the face during that phase of the design, I had no space to store the proposed layout in my basement with the full proscenium (shadow box) style with overhead LED lighting-built in. I have too many layouts in storage already. Fortunately, I realized that before construction started. The layout is going to have to be a simple shelf with a backdrop. This revised design also will save table material costs. **See the drawing below of One of Two Identical Foamcore Modules.**

Since operations is the main goal of the layout, I spent quite a bit of time thinking about the operations of the railroad. Questions I asked and resolved:

- Are the industries structures large enough to justify three or more freight car spots?
- Are there switching leads to switch out the industrial spurs?
- Are there “for now” tracks to use when switching?
- Are there Off Spot tracks?
- Are the passing sidings long enough for (9) car trains (plus engine and caboose)?
- Are there cleanout tracks for flour and grain cars?
- Can the grain doors be installed on the cleanout tracks?
- Are there yard tracks for ordering/blocking cars before switching?
- What are the car requirements for each industry?
- Is there enough track capacity between the clearance points to meet the operations objectives for that track?
- When the 1 : 1 structures are reduced to N-scale, how much size reduction is required to fit the layout? The Char-Gale Warehouse is 566ft long, or 42” long in N-scale. Is it still believable if shortened to 36” long on the layout?
- Coming from the staging/fiddle track onto the layout, what track will the train be pulled onto?
- Is there room for the train to be broken down/classified into spots, off spots, and cleanouts?
- Where will the pulled cars be set while the spots are being placed?
- Are the pulls to be put into a block for the outgoing train, or are they to be added to the outbound train as they are pulled?
- Create a Sequence Schedule to outline what trains are to operate and in what order. A Sequence Timetable eliminates the time factor. It allows work to take place without any pressure. It allows time for talking and explanations during operations at the Train Shows.
- Make schematics of each industry labelling the spots, type of cars at each spot, where the closest cleanout track is, and location of the off-spot track is for that industry.
- Write out clear and concise job assignments. They should include job priorities, CC/WB procedures, special instructions, if any, for a specific industry site, and locomotive train length limits.
- Remember the combined track design and operations design is provided for the entertainment for the traing crew (engineer and conductor).

Last month I included a schematic of a possible layout (eighth sketch). After answering the above questions about the design and the possible operations, I changed the design. The ninth sketch offered more variety in structures and operations. **See the North End (Left) Module and South**

**End (Right) Module sketches below.** Instead of two very large flour mills and a large grain elevator on the previous design (all were large cast concrete structures that took up the full right module), the new design uses an elevator that is a wooden structure covered with corrugated iron, a cast concrete small feed and flour mill, a seed company that is constructed of brick and concrete, and a multi-structure lime and cement warehouse business. These also offer more variety in freight car selection. Though, in 1958, most freight was hauled in boxcars. Below is a basic description of the industries from the corridor I selected to model on this layout.

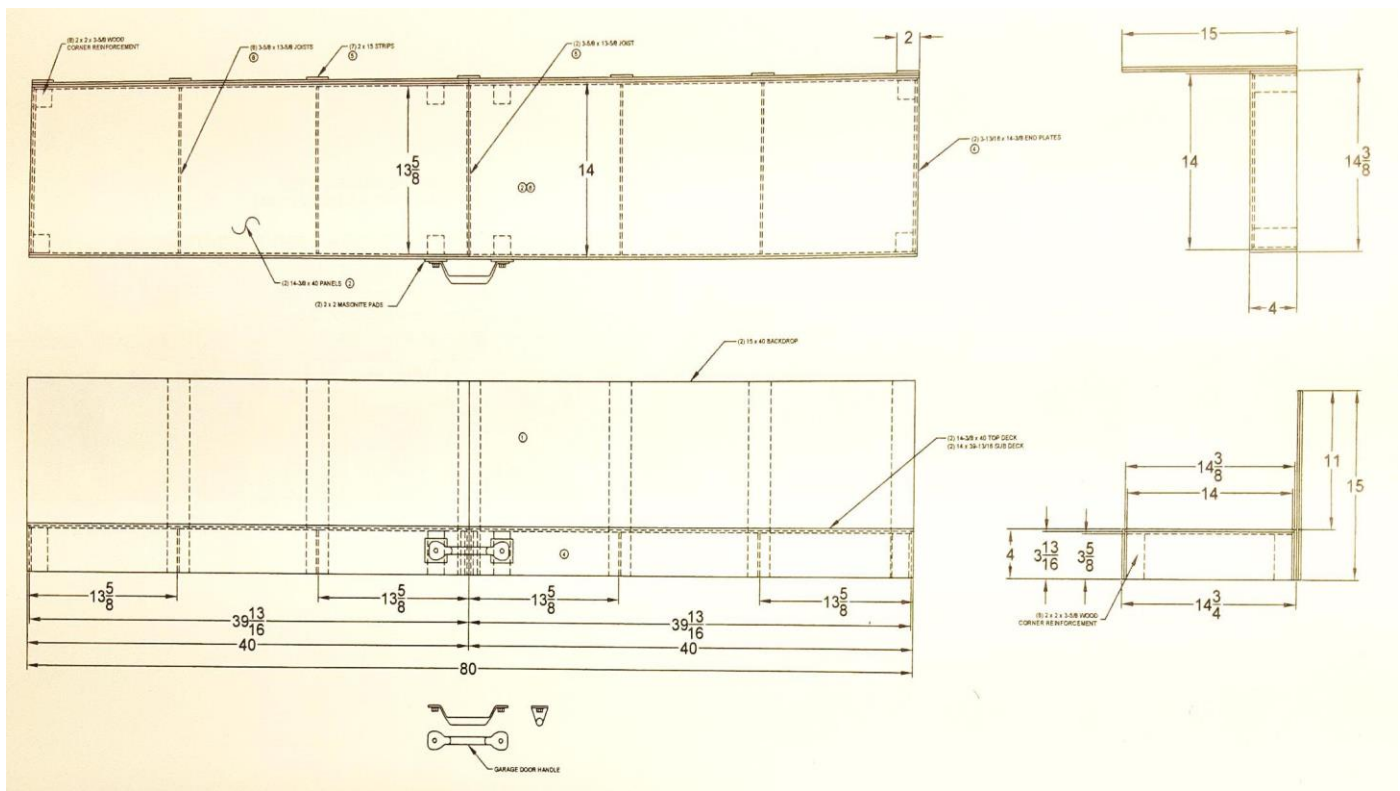
- At the North (left) end of the layout is the Toro Manufacturing Co. Toro was building tractors and golf course cutting equipment. It was built in 1919 and torn down in November 2023. It receives hoppers of coal for its boilers, boxcars of sheet metal, rough castings, wire, paint, manufactured components, tools, tires, etc. It shipped replacement parts, tractors, and large reel mowers. Toro requires flat cars, boxcars, gondolas and hoppers to receive and ship goods. Their buildings have four spots.
- Across the track from Toro is E.E. Bach Millwork Co. They manufacture and ship doors, windows, door and window trim, crown molding, etc. They receive wood, glass, rope, cutting tools, cast weights, hardware, etc. in boxcars. Their siding holds three 40ft boxcars.
- ACME Foundry is next to Bach Millwork. They create and ship light gray iron castings both raw and machined for customers. They receive lumber, green sand, iron ingots, machine tools, etc. They require covered hoppers, boxcars, gondolas, and flat cars. Adjacent to the foundry is an electric power substation to provide the energy required to melt the iron. The siding has room for three cars.
- Wheeler-Barnes Co is a producer of syrups, jellies, preserves, and peanut butter. It is located to the south (right) of Toro. It has a spur with five spots for receiving sugar, corn syrup, coal, fruit, peanuts, jars, lids, cardboard, labels and salt. Their spur holds five cars.
- The largest structure on the layout is Char-Gale Manufacturing Co. This is Warehouse No. 3. It was built in 1946. They are a sheet metal and furnace manufacturer selling products used in HVAC. They ship and receive parts daily. Their siding can handle four 50ft boxcars.
- Van Dusen-Harrington Co is part of Monarch Elevators. Elevators #1 and #2 were built in 1888 and 1894. They were large wooden structures that were later covered with corrugated iron. They were the only wood/corrugated iron elevators on the Hiawatha corridor. All others were cast concrete. The machinery was steam powered. Coal was delivered for the boilers.

Maintenance parts, belts, buckets, grease, bearings, etc. were also received in boxcars. Wheat was shipped to the flour mills. The spur can handle five 40ft boxcars of grain and one hopper of coal.

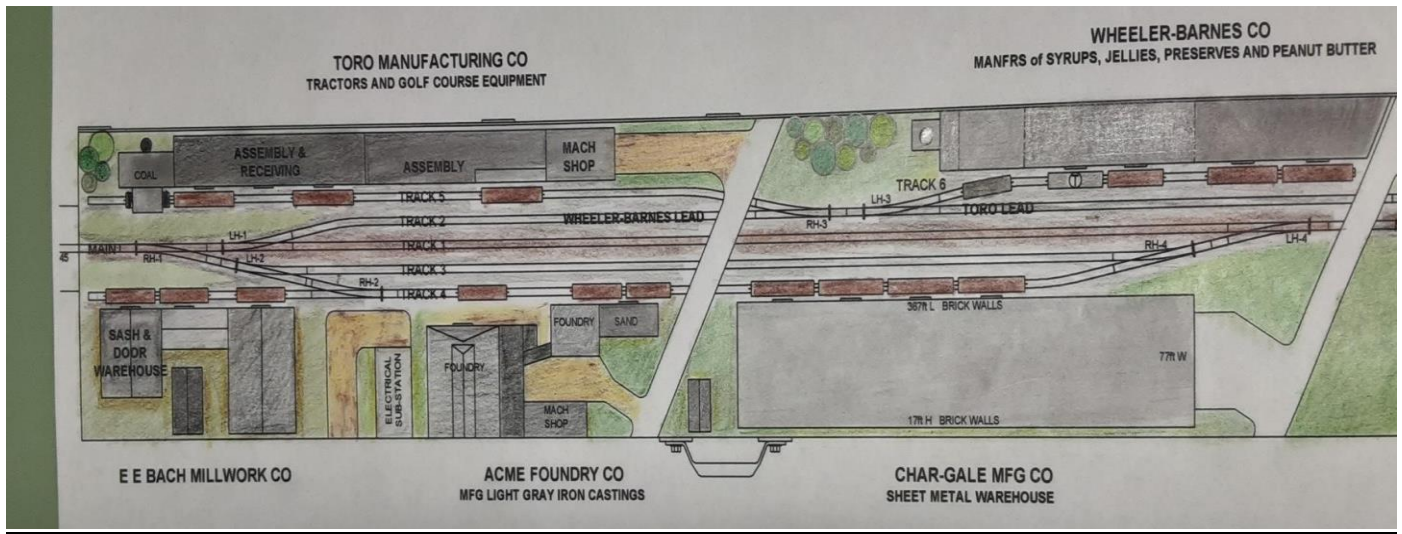
- Landers-Norblom-Christenson Co is across from Van Dusen. It is a lime and cement warehouse company selling products to the building contractors in the area. They received bagged lime, bagged cement, and bricks in boxcars. Tank cars of gasoline were unloaded into storage tanks for redistribution. The spur can hold five cars, four boxcars and one tank car.
- Minneapolis Seed Co was part of Cargill Elevator Inc. They packaged and shipped wheat, seed corn, sunflower seeds and other grains. The seeds were shipped in burlap bags and paper seed packages. Their spur can hold eight boxcars.
- Priority Mills is a smaller feed and flour mill. Milled flour is bagged and shipped. Wheat middlings are pelletized, bagged and shipped for livestock feed.

In all there are 37 possible freight car spots. If I operate with 18 to 20 cars on the layout and two nine car trains, I should be kept busy switching for several hours.

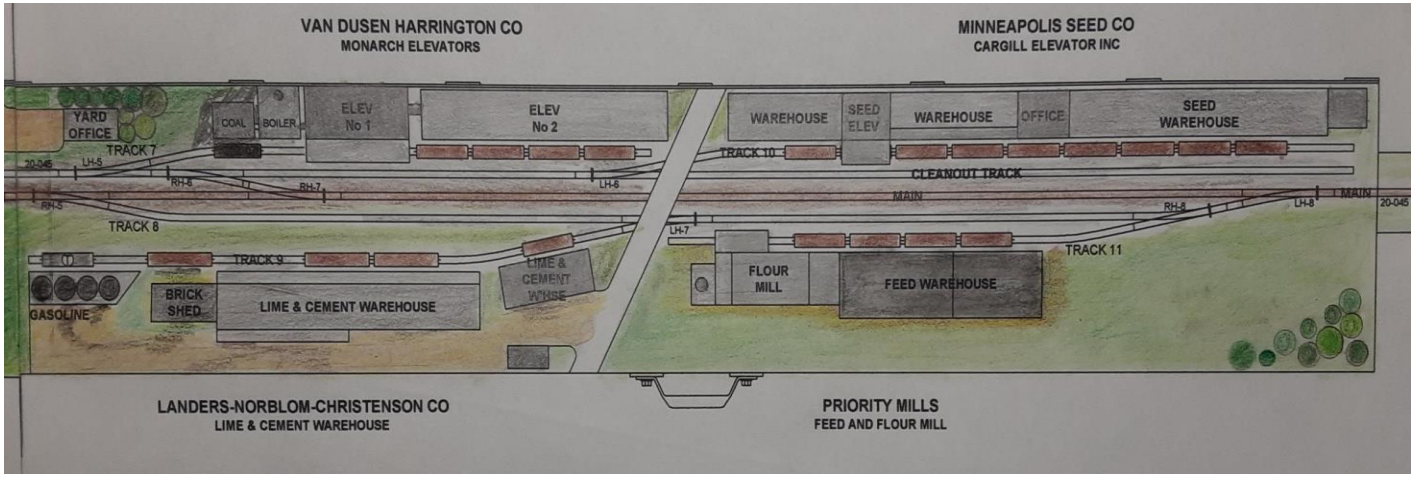
I have started construction on the foamcore modules. Next month I will describe and show pictures of how I built the modules.



**One of Two Identical Foamcore Modules**



**Corridor North End Module (Left Side)**



**Corridor South End Module (Right Side)**